

**Kingsland Royal Estates
PLANNED DEVELOPMENT (PD) DISTRICT**

**Amendment to the Zoning Ordinance
City of Kingsland, Georgia**

**DEVELOPMENT
PLAN**

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I. GENERAL PROJECT DESCRIPTION

The applicant proposes to amend the City of Kingsland Zoning Ordinance to create a new Planned Development (PD) District for approximately 550 gross acres of land abutting state road 40 on the north and Oakwell Church Road on the south, specifically described in the attached legal description. This property is located outside of the existing corporate City limits and is requested to be designated a PD District upon annexation by the City of Kingsland, Georgia.

The development, named Kingsland Royal Estates, is designed to promote a balance of community involvement and private life in an affordable, attractive and sustainable setting. Plan elements include a diversity of housing types, ranging from small to large lot single family residential, multi family apartments and townhomes. There will be an active public realm in the commercial center, a variety of community and recreational amenities, land available for a public school site and a network of picturesque streets, trails and open space. Kingsland Royal Estates was master planned in conjunction with the *Quality Community Objectives as established by the State of Georgia's Standards and Procedures for Local Comprehensive Planning*, employing a range of unifying features such as smart growth design, neotraditional neighborhood development standards, integrated land uses and pedestrian oriented streetscapes.

As outlined in *Section 66, Planned Unit Development District*, "The purpose of a Planned Unit Development District is to offer developers the benefits of efficiency, economy, and flexibility by encouraging unified development of large sites, while deriving for the City the advantages of improved appearance, compatibility of uses, optimum service by community facilities and better handling of vehicular access and circulation". Further, per *Section 71, Planned Development District*, "The purpose of this district is to achieve site design and land development of superior quality through the encroachment of flexibility and creativity in achieving the purposes of other districts in this Zoning Ordinance by departing from strict application of use and dimensional requirements under certain conditions and imposing other requirements in lieu thereof. The application of PD zoning for Kingsland Royal Estates proposes to meet both requirements as outlined within this zoning document. This project is unique and may differ from other projects developed within Kingsland. This document is based on the 2002 adopted zoning regulations and the City's zoning and subdivision regulations in force at the time of adoption of this PD zoning shall govern the current as well as future phases and modifications and shall bind this development as proposed here within and as adopted by the City. Development criteria not addressed within this PD shall comply with *Kingsland Zoning and Land Development Ordinance* or other local, state and federal criteria.

II. LEGAL DESCRIPTION

See attachment A for project boundary map and legal description.

III. ZONING MASTER PLAN

Kingsland Royal Estates is a mixed use community that will be developed incrementally over an anticipated period of 5 to 10 years, market depending. The project may be sold to one Development Company that can implement all aspects of the development, or the project may be sold to individual developers/builders who specialize in specific product types and uses. Flexibility becomes important to allow the development pattern to move forward while assuring the City of Kingsland and master developer predictable and controlled development. Of primary importance is the establishment of development density, compatibility of land use allocations and the identifying any specific criteria specific to this development. A clear PD document will allow for an efficient submittal and review process for all parties.

A. Development Density

The overall development pattern can most successfully be regulated by the density of development allowed. As stated, flexibility is important, yet density is one tool that can regulate the pattern for development to insure a consistent and compatible development pattern while allowing flexibility in execution. Within Kingsland Royal Estates, there are two distinct development zones, the residential development zone and mixed use development zone. The zones establish the big picture for the development of the project by prescribing land use types and densities allowed, as described below. The more detailed site development regulations outlined within this PD document all originate from these zones.

Land Use Types/ Development Zones	Commercial Center	Village Center	Neighborhood General	Neighborhood Edge
Residential Use Development Zone				
Mixed Use Development Zone				

A.1 Residential Use Development Zone

A.1.1 The primary land use within Kingsland Royal Estates will be residential. There is a variety of residential housing types allowed, ranging from larger single family lot to medium lots down to small "TND" sized lots. Townhomes and Multi Family apartment complexes are also allowed. Flexibility is important for proper distribution of the various land uses within the site. To allow flexibility, yet control the overall density, the maximum density allowed on the project's 550 overall acres shall not exceed the density as prescribed in the City of Kingslands R-2 Low Density Residential District of 4.3 dwelling units (du) per acre or 2,365 residential units. As an example, an apartment complex may be successfully developed at 24 du/acre, townhomes at 8 du/acre and single family homes at 4 du/ac, as long as the overall units at build-out remain under 4.3 du/ac. Acreage for commercial/office use shall be subtracted from the overall site area in determining residential density, therefore, the 4.3 du/ac is based on residentially developed acres. Development standards as outlined within this PD shall apply.

A.2 Mixed Use Development Zone

A.2.1 Other land uses within Kingsland Royal Estates includes mixed land uses. Mixed land uses include commercial (retail), office, civic and limited residential land uses. There are two levels considered when applying the mixed land uses to the project, the Commercial Center and Village Center. There is no density cap to the development of

commercial/office space, as long as all development guidelines are met. The acreage for commercial/office use shall be deducted from the overall residential density calculation.

- a. The commercial centers shall be allowed to occur within a 2,000 foot corridor parallel to Highway 40 and/or Oakwell Church Road. The general intent of the commercial center is to provide larger scale, regional office and retail services to the community at large, therefore located along existing roadways that serve not only Kingsland Royal Estates, but the City of Kingsland outlying areas. Specific higher density residential uses are allowed within the commercial center as outlined further within this PD.
- b. The Village Center can occur anywhere on the site and integrates less intense office and commercial uses, excluding “big box” retailers and drive through facilities. This allows for integration with less dense residential development, such as townhome and single family residential. Additionally, civic uses, which include, but are not limited to schools, churches and community centers, may be located throughout the site. Specific uses and development standards are outlined within this PD.

A.3 Master Utility Development

To allow for the proper implementation of the allowed land uses, “utilities” (defined to include, but not be limited to water and sewer services, storm water management, electricity, phone, cable, street lighting, etc.) and “development standards” (defined to include, but not be limited to wetland delineation and protection, park and recreation implementation and development standards as outlined here within, etc.) shall be designed and implemented in a comprehensive, cohesive manner. Development can occur anywhere on the site as long as utilities are sized to accommodate tracts of land that the development at the time may have passed or “skipped over”. In a similar fashion, setbacks, road infrastructure and green space shall accommodate the surrounding development as well.

A.4 Density Tracking

To properly track the allowable densities, upon application for preliminary site plan review for any portion/application for development of Kingsland Royal Estates, the applicant shall submit a tabulation of the proposed residential and non-residential land uses for the subject site plan. The tabulation shall reflect the overall densities allowed tracked against densities that remain available for development. This shall be in addition to all other site plan review development criteria as outlined here within the Kingsland Royal Estates PD and per the City of Kingsland Development Standards.

A.5 Existing Land Use

The Kingsland Royal Estates property is currently utilized for forestry operations/timber production. Undeveloped portions of the site will remain in forestry production with silviculture exemptions for applicable land management regulations remaining in place until such time that a particular tract is platted for subdivision.

A.6 Development Standards

This section outlines comprehensive density strategies for development and overall land use control (Master Development). Detailed development standards are outlined throughout the PD document and shall be applied comprehensively to the successful development of Kingsland Royal Estates.

IV. STANDARDS FOR DEVELOPMENT

A. Restrictions on the use of property

The Kingsland Royal Estates Planned Development (PD) District is authorized pursuant to Sections 66 and 71 of the City of Kingsland Zoning Ordinance, and other Articles and Sections referenced therein, as amended. A separate special permit is not necessary or required to permit any use that is herein allowed within this PD district.

A.1 The Conceptual Site Plan

The conceptual site plan is a visual reference map of Kingsland Royal Estates and does not specifically address all development guidelines as outlined here within. The conceptual site plan provides a broad conceptual plan and vision for the development that illustrates the general land use arrangement and neighborhood connectivity. Elements such as utilities, transportation improvements, specific land uses, and open space and recreation amenities, to name a few, will be more fully designed and engineered to reflect the intent of the conceptual site plan's vision and incrementally submitted with detailed site plans at the time of future site plan reviews and permitting.

After approval of the Kingsland Royal Estates PD District, preliminary and final plats, building permits, and certificates of occupancy shall be issued and approved when in compliance with the conceptual site plan as approved, or as amended, and other required development regulations are met.

A.2 Use Restrictions

The detailed restrictions on the use of the property will be identified here within and remain consistent with the vision of the conceptual site plan. The regulations are designed to allow flexibility, yet ensure a level of predictability for internal site planning and the arrangement of various land use elements. The use restrictions are further detailed in the next section.

A.3 Density

The density, yard and height requirements will be governed by Transect Zones and corresponding Lot and Building Standards illustrated here within. These requirements are further detailed in the next section. The maximum residential density for Kingsland Royal Estates shall not exceed 4.3 du/ac, the maximum density allowed under R-2 zoning.

A.4 Natural Environment

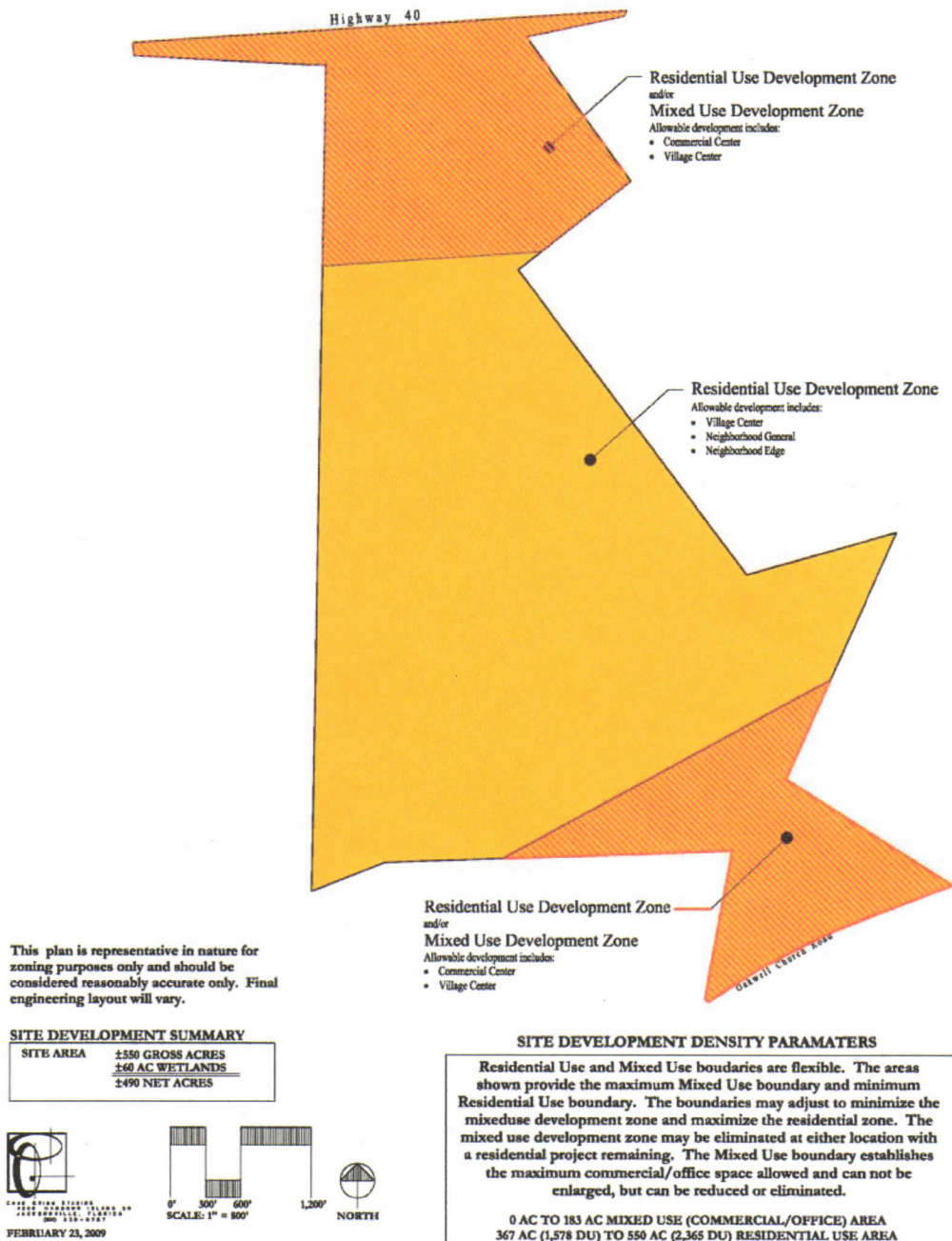
Development within Kingsland Royal Estates will occur in a way that minimizes the impact upon the natural environment. Preserving wetland systems and maintaining the natural hydrology of the site are two of the methods that will be employed to preserve these natural resources. Upland buffers are proposed that will protect the wetland systems and provide opportunities for community green space and trails that allow for interconnecting the neighborhood.

The majority of the site is relatively flat and will assumedly need to be elevated to achieve the ability to retain water prior to release into the wetland or other conveyance systems. Stormwater will be retained in compliance the City of Kingsland *Zoning and Land Development Ordinance*. Erosion control methods will be implemented in compliance with the City of Kingsland's *Minimum requirements for erosion and sedimentation control best management practices, Section 182*.

KINGSLAND ROYAL ESTATES

PD CONCEPTUAL PLAN FOR
A MIXED USE COMMUNITY

Kingsland, Georgia



B. Density, Yard and Height Requirements

B.1 The Transect Zoning Classification

Kingsland Royal Estates Planned Development District is organized on the Transect Zoning Classification. The Transect is a planning categorization system that organizes the elements of the built environment on a sliding scale that ranges from rural to urban. The Transect provides a regulatory structure that utilizes zoning categories based on the concept for developing the physical form of the built environment that develops intermixed and immersive environments historically common in towns and cities. Here, all of the elements of the human environment work as one self-sustaining, interdependent subsystem, yet each with distinct characteristics and behavior pattern.

The Kingsland Royal Estates PD District contains four Transect Zones, ranging from neighborhood edge to the village center, generally described as follows:

B.1.1 The Neighborhood Edge zone, the largest zone for this development, is residential in character. The Neighborhood Edge includes a range of single family detached housing types on medium to large lots. Streets range from narrow and lined with shade trees, to boulevards, where houses front larger green spaces. Parks and green space are integrated within the road network and neighborhoods. Sidewalks and/or trails on both sides of the street, along with street front focused architecture allow more opportunity for community socialization.

B.1.2 The Neighborhood General zone is primarily residential in character with an urban influence. The Neighborhood General includes a range of housing types including single-family detached housing and single family attached (townhouses). Streets are narrow and lined with shade trees and alley access is more common. Sidewalks and/or trails on both sides of the street, along with street front focused architecture allow more opportunity for community socialization.

B.1.3 The Village Center zone is a mixed use zone, often located around a neighborhood park, lake or other common community feature. The neighborhood center may include parcels that are primarily residential, primarily commercial, or a mix of both. The residential housing types may range from small lot single family detached, single family attached to multi-family residential. Retail, office and civic uses complement the higher density residential uses and may be combined with the residential in live/work types of units. The streetscape provides for on-street parking to the largest extent possible. Public spaces include plazas and encroachments into the sidewalks. Architectural control, site furnishings, hardscape elements, and planted trees provide for a dynamic center to this project and surrounding community. The Village Center allows for great flexibility in development use types, intermixed within one special core district.

B.1.4 The Commercial Center zone will be the development's commercial core. A mixed-use zone, it allows for a mix of civic, retail and office uses. Located along highway 40, big box retail and multi tenant commercial/office that require large fields of parking may be developed within this zone. The commercial center serves as a transition between the pedestrian oriented village center and the vehicular orientation to highway 40. As such the buildings are oriented to the street that is developed to combine both higher volumes of traffic and pedestrian movements as one cohesively developed and safe environment.

THE TRANSECT ZONE MATRICES

Land Use Types Matrix

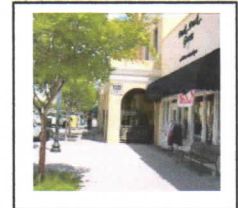
The Land Use Types Matrix describes the permitted uses within each Transect Zone, complementary to the goals of developing a diverse, attractive and healthy community. Separate uses may be combined or mixed on a single lot in two ways. The first is vertical combination, where two different uses, such as retail on the first floor and housing above, are combined in one building. The second type of use integration is horizontal, where two uses may be attached by a common wall, or detached on a single lot. For example, a professional office might be located along the side of a house, or in a garage in the rear. The combination of allowed uses is presented in an easy to understand graphic matrix, with additional clarifying notes attached.

Kingsland Royal Estates features a mix of uses, all complementary to the goal of developing a healthy, affordable community instead of disconnected “pods” of single-use activity. While the Commercial Center provides for the majority of commercial uses, the Village Center zone allows a range of land uses. The Neighborhood General zone allows primarily residential development and home based business while the Neighborhood Edge zone is residential.

In parenthesis under each land use (where applicable) is the City of Kingsland comparable land use designation. The land uses within this document are generalized and the detailed uses as outlined within the current zoning for each designated land use shall guide the specific land uses allowed. Detailed descriptions are outlined in the Kingsland Zoning Ordinance, Article VI and as outlined within the definitions within the Glossary of this PD document.

Shaded = Permitted Use (See notes) Not Shaded = Prohibited Use

Transect Zones/ Land Use Types	Commercial Center	Village Center	Neighborhood General	Neighborhood Edge
Civic or Community				
Retail/Office (C-1)			See Note 1	
Retail/Office (C-2)				
Live-Work Unit				
Residential – Single Family (R-1)				
Residential – Single Family (R-2)				
Residential – Medium and High Density (R-3)				
Carriage House		See Note 2	See Note 2	
Recreation	See Note 3	See Note 3	See Note 3	See Note 3
Accessory Buildings and Structures				



Notes:

1. Retail, office and service uses are permitted but limited to the first floor of a building up to a maximum 3,000 sf per unit. Residential use is permitted above the retail, office or service use.
2. Carriage houses shall not exceed 1,200 sq. ft. in floor area. Carriage houses shall be located within an Accessory Building to the rear of the primary building and shall have separate outside entrances.
3. Recreation uses may include park and recreation elements, including, but not limited to athletic fields, multi-purpose playing fields, picnic areas, playgrounds, trails and passive green space with accessory and supportive uses to them.

Lot & Building Types Matrix

The Lot & Building Types Matrix establishes the development criteria for Lot & Building Types within each Transect Zone. The matrix indicates which building types, described in the Lot & Building Standards below, are permitted with certain conditions. The goal is to provide for a variety of buildings in each zone while fostering compatibility and an overall coherent development pattern.

Kingsland Royal Estates provides a variety of lot and building types, including large, medium and small single family detached homes, townhouses, apartments, condominiums and live work units. The mix of uses and building types provides the choices needed to create a community where people of different ages, incomes and families can live, work and play. The standards for each Lot & Building Type are explained in detail on the following pages.

Shaded = Permitted (See Notes) Not Shaded = Prohibited

Transect Zones/ Lot & Building Types	Commercial Center	Village Center	Neighborhood General	Neighborhood Edge
Type A1 – Small lot detached SF				
Type A2 – Medium lot detached SF				
Type A3 – Large lot detached SF				
Type A4 – Estate lot detached SF				
Type B – SF Attached (Townhome)				
Type C – Multi Family/Civic				
Type D – Commercial	See Note 1	See Note 1	See Notes 1&2	

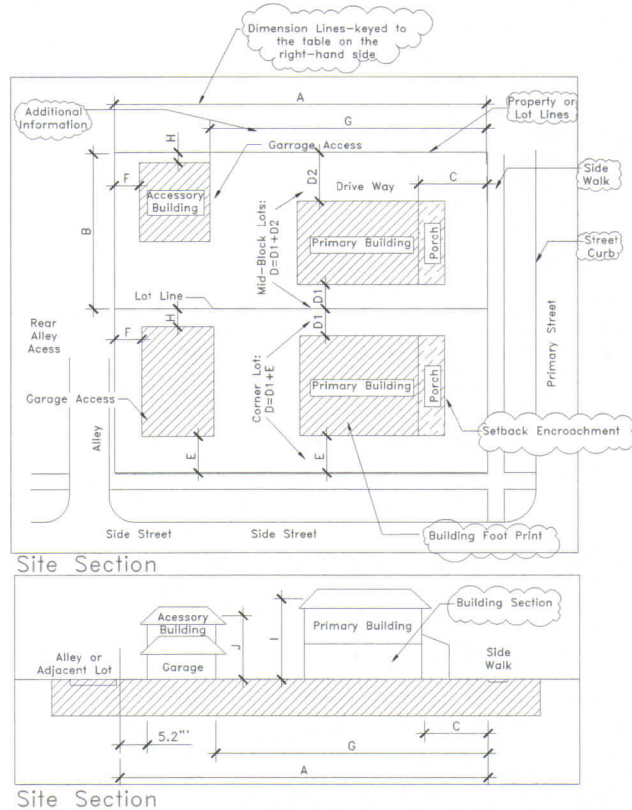
**Notes:**

1. Mixed-use buildings may occupy two or more of the following uses within the same building: retail, professional services, personal services, workshop, office, artisan, restaurant, lodging, childcare, professional business, government services, entertainment, recreational, residential and related accessory uses. Uses may be divided vertically and/or horizontally.
2. Retail uses are limited to 3,000 sf maximum within the first floor of building only. Permitted uses include retail sales, personal services (such as hair salon), professional services, offices, artisan, coffee shops and related accessory uses.

LOT & BUILDING STANDARDS

How to Interpret Lot Diagrams

The Kingsland Royal Estates Lot & Building Standards are designed to be user-friendly and understandable by developers, regulators, homebuilders and homeowners. Each building type is detailed in individual pages through diagrams and text, both establishing the required development criteria. Photos are also provided to assist in illustrating the intent of the requirements.



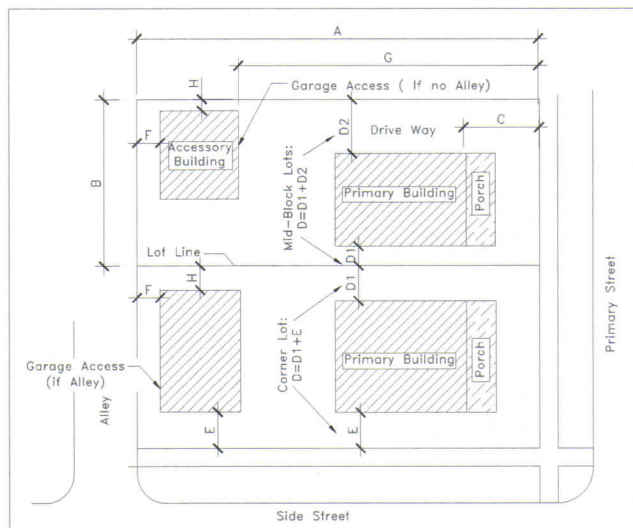
Lot & Building Standards (“Building Standards”) establish basic site and building design features. Each lot & building type describes the physical constraints on the placement and configuration of three-dimensional building forms and façade elements, such as porches and balconies, on the lot. The Building Standards include detached, Accessory Buildings such as garages, and describe spaces required for vehicle parking.

This diagram represents a site-section through the lot, and is primarily used to represent building heights. It is also useful for illustrating the lot standards three-dimensionally.

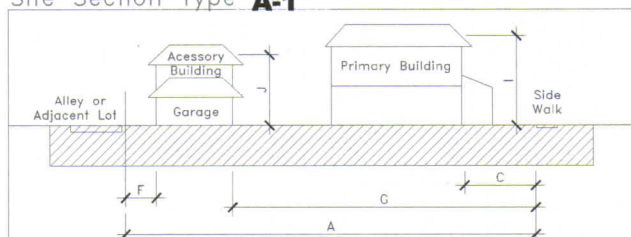
Type X Lot Standards	
A. Minimum Lot Depth	x ft.
B. Min. Lot Width (each unit)	x ft.
C. Front Yard Build To Line	x ft.
D. Min. Combined Side Yard Adjacent Lot Setback	x ft.
E. Side Street Build to Line	x ft.
F. Min. Primary or Accessory Bldg. Rear Setback	x ft.
G. Min. Accessory Bldg. Front Setback	x ft.
H. Min. Accessory Bldg. Side Setback	x ft.
I. Max. Primary Bldg. Height	x ft.
J. Max. Accessory Bldg. Height	x ft.
PARKING	
Min. Number of On-Site Parking Spaces per Dwelling Unit	x
Carriage House (Secondary Dwelling Unit)	Y/N
Y = Permitted; N = Prohibited	
Primary Building Entrance	
Note	
Allowable Encroachments:	
Note	
Note	
Note	

Type A-1: Small Lot Detached Single-Family

A-1 Small lot Single-Family Detached Houses are smaller detached residential units. They are located on lots that have useable side yards. Vehicular access may be provided by either alleys or front driveways, however, if alley access is provided, front driveways and street facing garages shall be prohibited. Attached or detached garages and shared driveways are permitted.



Site Section Type **A-1**



Site Section Type **A-1**



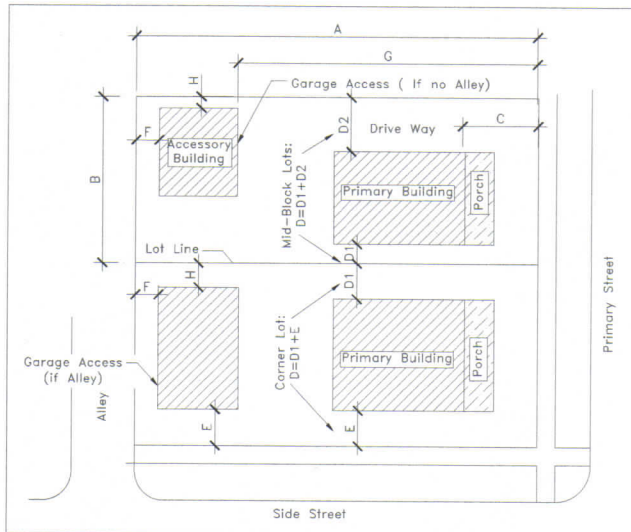
Representative SF Detached Residence

Type A-1 Lot Standards

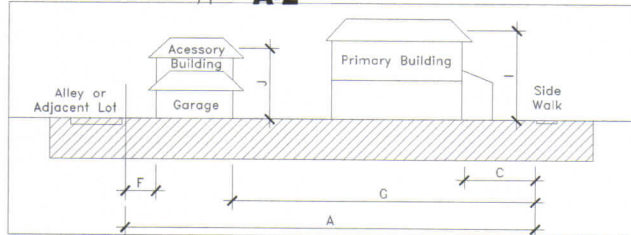
A.	Minimum Lot Depth	90 ft.
B.	Min. Lot Width	38 ft.
	Min. Flag Lot Width	25 ft.
	Minimum Lot Area	3,420 sf.
C.	Front Yard Build To Line	15 ft.
D.	Min. Combined Side Yard Adjacent Lot Setback	7 ft.
	Setback shall be a minimum of 3' on one side, providing a minimum 6' clear between residences. If the 3' minimum utilized, AC units shall be located to the rear of the structure.	
	At corner lots, the side street Setback E shall be in the calculation of D.	
E.	Side Street Build To Line	5 ft.
F.	Min. Primary or Accessory Bldg. Rear Setback	Without Alley Access 8 ft. With Alley Access 20 ft.
G.	Min. Accessory Bldg./Garage Front Setback	45 ft.
H.	Min. Accessory Bldg. Side Setback	5 ft.
	This setback may be waived if Accessory Buildings are attached with the adjacent lot owners and comply with common wall fire rating criteria.	
I.	Max. Primary Bldg. Height (2.5 stories)	36 ft.
	Minimum Finished Floor Height is 1.0 ft from crown of road.	
	Non-living space tower elements of a maximum of 200 sq. ft. in floor area shall be exempt of the height limit and may exceed the ridge height of the primary roof by a maximum of 10 ft.	
J.	Max. Accessory Bldg. Height	30 ft.
K.	Minimum Dwelling Unit Size	900 sf.
	PARKING	
	Minimum number of on-site parking spaces per primary dwelling unit	2
	Minimum on-site parking for carriage houses (access from same means as primary unit)	1
	Shared Driveways encouraged for front loaded garages	
	Carriage House	Y*
	* Shall be limited to 1,000 sq. ft., limited to owner occupied residences only.	
	Primary Building Entrance:	
	The primary pedestrian building entrance shall be located along a public street, common walkway or park.	
	Allowable Encroachments:	
	Covered or uncovered porches or stoops that are unenclosed (except with screens) may encroach into the front setback no deeper than 10 ft. and no closer than 5 ft. from a property line.	
	Patios may encroach into rear and side setbacks, no deeper than 8 ft. and no closer than 3 ft. from a property line.	
	Driveways may encroach into all setbacks where curb cuts are permitted.	
	Y = Permitted; N = Prohibited	

Type A-2: Medium Lot Detached Single-Family

A-2 Medium Lot Single-Family Detached Houses are mid-sized detached residential units. They are located on lots that have useable side yards. Vehicular access may be provided by either alleys or front driveways, however, if alley access is provided, front driveways and street facing garages shall be prohibited. Attached or detached garages and shared driveways are permitted.



Site Section Type **A-2**



Site Section Type **A-2**



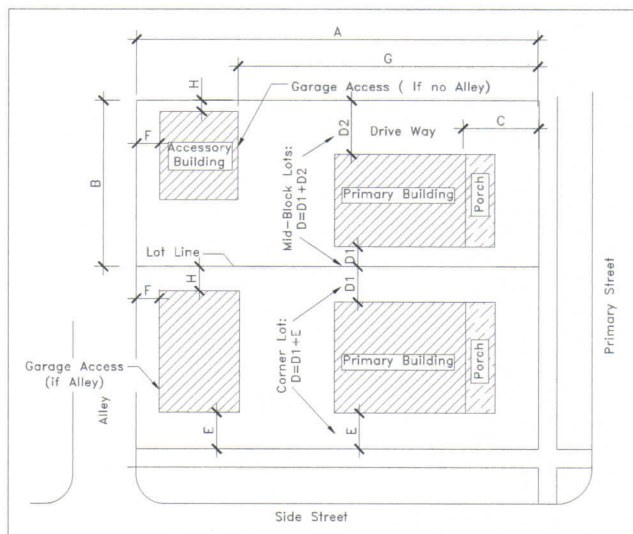
Representative SF Detached Residence

Type A-2 Lot Standards

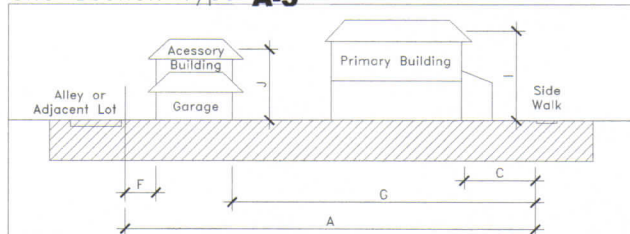
A.	Minimum Lot Depth	90 ft.
B.	Min. Lot Width Min. Flag Lot Width	48 ft. 25 ft.
	Minimum Lot Area	4,320 sq. ft.
C.	Front Yard Build To Line	15 ft.
D.	Min. Combined Side Yard Adjacent Lot Setback	10 ft.
	Setback shall be a minimum of 4' on one side, providing a minimum 8' clear between residences. If the 3' minimum utilized, AC units shall be located to the rear of the structure.	
	At corner lots, the side street Setback E shall be in the calculation of D.	
E.	Side Street Build To Line	5 ft.
F.	Min. Primary or Accessory Bldg. Rear Setback	Without Alley Access 8 ft. With Alley Access 20 ft.
G.	Min. Accessory Bldg./Garage Front Setback	45 ft.
H.	Min. Accessory Bldg. Side Setback	5 ft.
	This setback may be waived if Accessory Buildings are attached with the adjacent lot owners and comply with common wall fire rating criteria.	
I.	Max. Primary Bldg. Height (2.5 stories) Minimum Finished Floor Height is 1.0 ft from crown of road.	36 ft.
	Non-living space tower elements of a maximum of 200 sq. ft. in floor area shall be exempt of the height limit and may exceed the ridge height of the primary roof by a maximum of 10 ft.	
J.	Max. Accessory Bldg. Height	30 ft.
K.	Minimum Dwelling Unit Size	900 sq. ft.
	PARKING	
	Minimum number of on-site parking spaces per primary dwelling unit	2
	Minimum on-site parking for carriage houses (access from same means as primary unit)	1
	Shared Driveways encouraged for front loaded garages	
	Carriage House	Y*
	* Shall be limited to 1,000 sq. ft., limited to owner occupied residences only.	
	Primary Building Entrance:	
	The primary pedestrian building entrance shall be located along a public street, common walkway or park.	
	Allowable Encroachments:	
	Covered or uncovered porches or stoops that are unenclosed (except with screens) may encroach into the front setback no deeper than 10 ft. and no closer than 5 ft. from a property line.	
	Patios may encroach into rear and side setbacks, no deeper than 8 ft. and no closer than 3 ft. from a property line.	
	Driveways may encroach into all setbacks where curb cuts are permitted.	
	Y = Permitted; N = Prohibited	

Type A-3: Large Lot Detached Single-Family

A-3 Large Lot Single-Family Detached Houses are larger detached residential units. Single-Family Detached Houses are located on lots that have useable side yards and the houses are allowed to be larger than on the A-1 and A-2 lot. Vehicular access may be provided by either alleys or front driveways, however, if alley access is provided, front driveways and street facing garages shall be prohibited. Attached or detached garages and shared driveways are permitted.



Site Section Type **A-3**



Site Section Type **A-3**



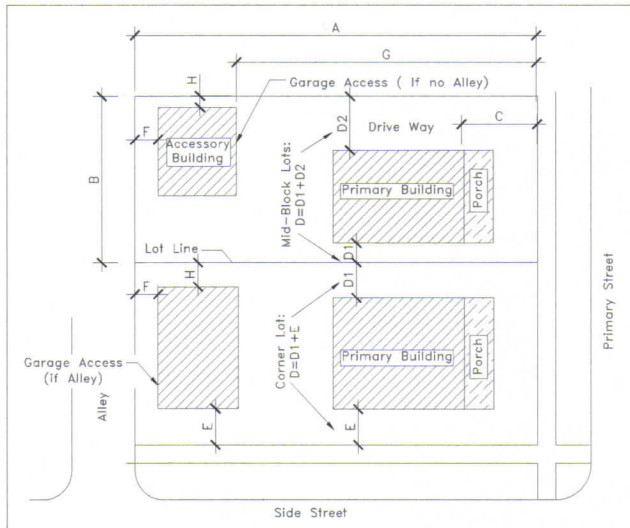
Representative large lot SF Detached Residence

Type A-3 Lot Standards

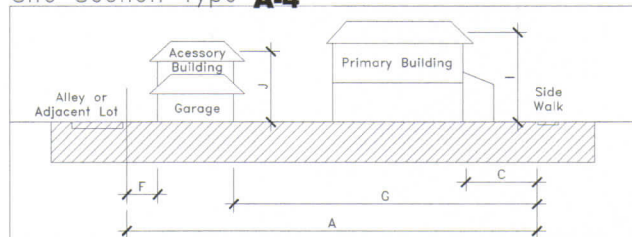
A.	Minimum Lot Depth	115 ft.
B.	Min. Lot Width	60 ft.
	Min. Flag Lot Width	25 ft.
	Minimum Lot Area	8,000 sf.
C.	Front Yard Build To Line	20 ft.
D.	Min. Combined Side Yard Adjacent Lot Setback	14 ft.
	Setback shall be a minimum of 5' on one side, providing a minimum 10' clear between residences. If the 5' minimum utilized, AC units shall be located to the rear of the structure.	
	At corner lots, the side street Setback E shall be in the calculation of D.	
E.	Side Street Build To Line	5 ft.
F.	Min. Primary or Accessory Bldg. Rear Setback	Without Alley Access 8 ft. With Alley Access 16 ft.
G.	Min. Accessory Bldg. Front Setback	45 ft.
H.	Min. Accessory Bldg. Side Setback	5 ft.
	This setback may be waived if Accessory Buildings are attached with the adjacent lot owners and comply with common wall fire rating criteria.	
I.	Max. Primary Bldg. Height (2.5 stories)	36 ft.
	Minimum Finished Floor Height is 1.0 ft from crown of road.	
	Non-living space tower elements of a maximum of 200 sq. ft. in floor area shall be exempt of the height limit and may exceed the ridge height of the primary roof by a maximum of 10 ft.	
J.	Max. Accessory Bldg. Height	30 ft.
K.	Minimum Dwelling Unit Size	1,000 sf.
	PARKING	
	Minimum number of on-site parking spaces per dwelling unit	2
	Minimum on-site parking for carriage houses (access from same means as primary unit)	1
	Shared driveways encouraged for front loaded garages	
	Carriage House	Y*
	* Shall be limited to 1,000 sq. ft., limited to owner occupied residences only.	
	Primary Building Entrance:	
	The primary pedestrian building entrance shall be located along a public street, common walkway or park.	
	Allowable Encroachments:	
	Covered or uncovered porches or stoops that are unenclosed (except with screens) may encroach into the front setback no deeper than 10 ft. and no closer than 10 ft. from a property line.	
	Patios may encroach into rear and side setbacks, no deeper than 8 ft. and no closer than 3 ft. from a property line.	
	Driveways may encroach into all setbacks where curb cuts are permitted.	
	Y = Permitted; N = Prohibited	

Type A-4: Estate Lot Detached Single-Family

A-4 Large Lot Single-Family Detached Houses are larger detached residential units on spacious lots. Single-Family Detached Houses are located on lots that have useable side yards and the houses are allowed to be larger than on the A-1, A-2 and A-3 lots. Vehicular access may be provided by front driveways Attached or detached garages and shared driveways are permitted.



Site Section Type **A-4**



Site Section Type **A-4**



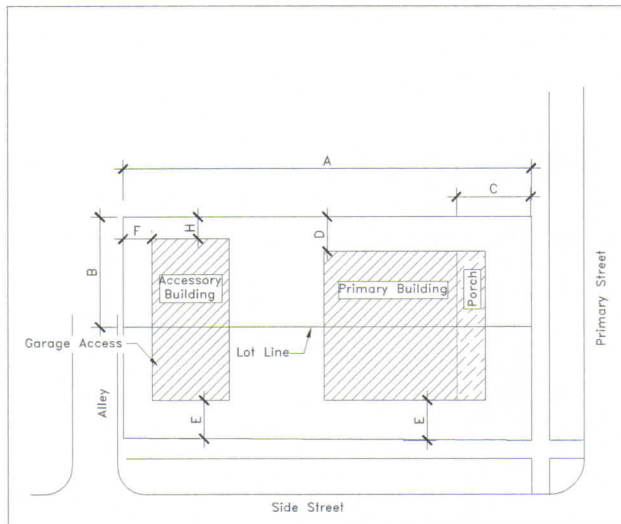
Representative Estate lot SF Detached Residence

Type A-4 Lot Standards

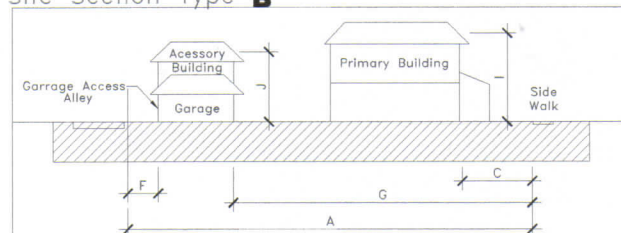
A.	Minimum Lot Depth	150 ft.
B.	Min. Lot Width	80 ft.
	Min. Flag Lot Width	25 ft.
	Minimum Lot Area	21,780 sf.
C.	Front Yard Build To Line	35 ft.
D.	Min. Combined Side Yard Adjacent Lot Setback	20 ft.
	Setback shall be a minimum of 5' on one side, providing a minimum 10' clear between residences. If the 5' minimum utilized, AC units shall be located to the rear of the structure.	
	At corner lots, the side street Setback E shall be in the calculation of D.	
E.	Side Street Build To Line	15 ft.
F.	Min. Primary or Accessory Bldg. Rear Setback	Without Alley Access 15 ft. With Alley Access n.a.
G.	Min. Accessory Bldg. Front Setback	55 ft.
H.	Min. Accessory Bldg. Side Setback	5 ft.
	This setback may be waived if Accessory Buildings are attached with the adjacent lot owners and comply with common wall fire rating criteria.	
I.	Max. Primary Bldg. Height (2.5 stories)	36 ft.
	Minimum Finished Floor Height is 1.0 ft from crown of road.	
	Non-living space tower elements of a maximum of 200 sq. ft. in floor area shall be exempt of the height limit and may exceed the ridge height of the primary roof by a maximum of 10 ft.	
J.	Max. Accessory Bldg. Height	30 ft.
K.	Minimum Dwelling Unit Size	1,800 sf.
	PARKING	
	Minimum number of on-site parking spaces per dwelling unit	2
	Minimum on-site parking for carriage houses (access from same means as primary unit)	1
	Shared driveways encouraged for front loaded garages	
	Carriage House	Y*
	* Shall be limited to 1,000 sq. ft., limited to owner occupied residences only.	
	Primary Building Entrance:	
	The primary pedestrian building entrance shall be located along a public street, common walkway or park.	
	Allowable Encroachments:	
	Covered or uncovered porches or stoops that are unenclosed (except with screens) may encroach into the front setback no deeper than 10 ft. and no closer than 25 ft. from a property line.	
	Patios may encroach into rear and side setbacks, no deeper than 8 ft. and no closer than 5 ft. from a property line.	
	Driveways may encroach into all setbacks where curb cuts are permitted.	
	Y = Permitted; N = Prohibited	

Type B: SF Attached

SF Attached units are houses on individual platted lots that share a common wall with one or more adjacent unit, up to a maximum eight attached units per building. Vehicular access may be provided by either alleys or front driveways, however, if alley access is provided, front driveways and street facing garages shall be prohibited. Attached or detached garages and shared driveways are permitted.



Site Section Type **B**



Site Section Type **B**



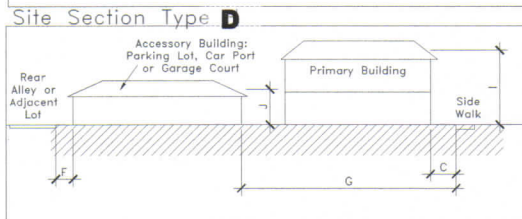
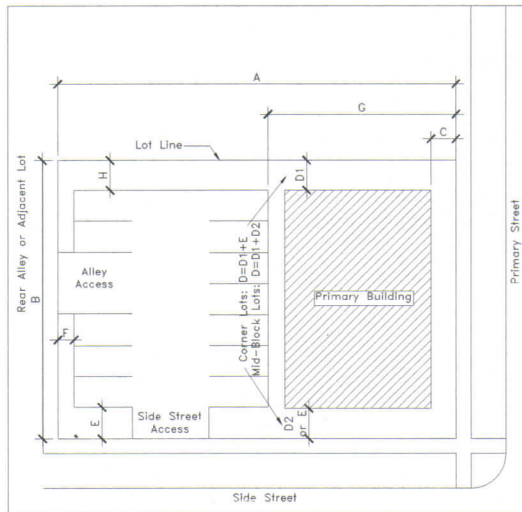
Representative Single-Family Attached/Town home

Type B Lot Standards

A.	Minimum Lot Depth	80 ft.
B.	Min. Lot Width where attached both sides	16 ft.
	Min. Lot Width on end unit/corner lot	25 ft.
	Minimum Lot Area	1,600 sf.
C.	Front Yard Build To Line	10 ft.
D.	Min. Combined Side Yard Adjacent Lot Setback	10 ft.
	Setback shall be a minimum of 5' on one side, providing a minimum 10' clear between residences.	
	Where buildings join, setback on attached side shall be 0	
	At corner lots, only the side street Setback E shall be applied.	
E.	Side Street Build To Line	5 ft.
F.	Min. Primary or Accessory Bldg. Rear Setback	8 ft.
	Without Alley Access	16 ft.
	With Alley Access	
G.	Min. Accessory Bldg. Front Setback	n/a
H.	Min. Accessory Bldg. Side Setback	5' ft.
	This setback may be waived if Accessory Buildings are attached with adjacent lot owners and comply with common wall fire rating criteria.	
I.	Max. Primary Bldg. Height (3 stories)	40 ft.
	Minimum Finished Floor Height is 1.0 ft from crown of road	
	Non-living space tower elements of a maximum of 200 sq. ft. in floor area shall be exempt of the height limit and may exceed the ridge height of the primary roof by a maximum of 10 ft.	
J.	Max. Accessory Bldg. Height	30 ft.
K.	Minimum Dwelling Unit Size	800 sf.
	PARKING	
	Minimum number of on-site parking spaces per dwelling unit	2
	Minimum on-site parking for carriage houses (access from same means as primary unit)	1
	Carriage House	Y*
	* Shall be limited to 1,000 sq. ft., limited to owner occupied residences only.	
	Primary Building Entrance:	
	The primary pedestrian building entrance shall be located along a public street, common walkway or park.	
	Allowable Encroachments:	
	Covered or uncovered porches or stoops that are unenclosed (except with screens) may encroach into the front setback no deeper than 8 ft. and no closer than 2 ft. from a property line.	
	Patios may encroach into rear and side setbacks, no deeper than 8 ft. and no closer than 3 ft. from a property line.	
	Driveways may encroach into all setbacks where curb cuts are permitted.	
	Y = Permitted; N = Prohibited	

Type D: Commercial

In the Commercial Center and Village Center zones, higher density, mixed use commercial development forms the commercial core of Kingsland Royal Estates. Commercial buildings, a common typology in small-town main streets, feature ground floor storefront retail, offices or service units with walk-up apartments or offices allowed on upper stories. Parking is located along the public street, in lots adjacent to the use or in the rear of the building. Alleys in the rear or side may provide vehicular access. Drive-throughs are permitted in the Commercial Center.



Site Section Type D

Allowed Encroachments in Public Right-of-Way

Outdoor Dining/Sidewalk Cafe

Upon approval of the City of Kingsland, a portion of the public right-of-way may be used where food and/or drink are served for consumption on the premises.

Minimum Requirements:

- A clear, unobstructed passageway not less than six feet in width at all points, across the frontage of the property parallel to the line of the street and generally in the line of pedestrian traffic shall be maintained at all times.
- The permitted area shall be separated from the pedestrian passageway with a fence or other rigid barrier having a minimum height of thirty-six inches but not greater than sixty inches except for necessary pedestrian ingress and egress
- Lease conditions as required by City of Kingsland.

Type D Lot Standards

A.	Minimum Lot Depth	70 ft.
B.	Min. Lot Width	25 ft.
	Minimum Lot Area	1,750 sf.
C.	Front Build To Line	0 ft.
D.	Min. Combined Side Yard Adjacent Lot Setback	0 ft.
E.	Side Street Build To Line	0 ft.
F.	Min. Primary Rear Setback	0 ft.
G.	Min. Accessory Bldg. Front Setback	NA
H.	Min. Accessory Bldg. Side Setback	NA
I.	Max. Primary Bldg. Height (3 stories)	45 ft.
	Tower elements of a maximum of 200 sq. ft. in floor area shall be exempt of the height limit and may exceed the ridge height of the primary roof by a maximum of 10 ft.	
J.	Max. Accessory Bldg. Height	NA
K.	Minimum Heated/Cooled Unit Size - Residential	650 sf.
	Commercial	1,300 sf.
	Maximum Lot Coverage	100%
	Min. number parking spaces per Dwelling Unit for residential	2
	Minimum number parking spaces per 600 sq. ft. of commercial uses	1
	Parking shall be accommodated on-site or off-site as part of a cross-parking easement. On-Street parking is allowed to be counted in the cross-parking easement.	
	Carriage House	Y
Y = Permitted; N = Prohibited		
Primary Building Special Considerations:		
The primary pedestrian building entrance shall be located along a public street or private street, common driveway, common walkway or park.		
First floor use required to be commercial/office. Second and subsequent floors may be commercial, office or residential.		

Representative Commercial



ALLOWABLE STREET TYPES BY TRANSECT ZONE

To insure the successful development of the Kingsland Royal Estates community, it is also critical to develop another organizational element, which is addressed through the development of Standards of Street Types within each transect zone. Arguably one of the most important elements to the success of a diverse community, the street is the public space of a community where vehicles and pedestrians must successfully and safely interact. This is where neighborhood socializations ensue and the image of the community is presented. Utility networks traverse the grid below ground and the interaction with the environment co-exists.

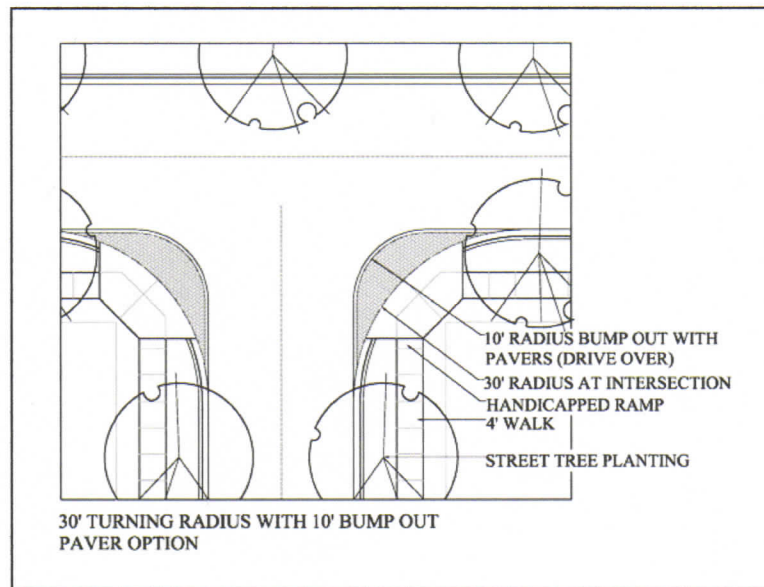
The following present general standards that apply to all street types as well as specific criteria for the different street types allowed within the development of the Kingsland Royal Estates PD.

GENERAL REQUIREMENTS FOR STREETS AND OTHER RIGHTS-OF-WAY

Street and right-of-way (ROW) shall comply with Sections 161 through 165 and sections 172 through 174, *Kingsland Zoning and Land Development Ordinance*, current edition at time of PD approval. In addition the requirements of this section, the following additional criteria/exceptions shall govern.

A. Corner Radii & Clear Zones:

Corner curb radii shall be between 10 feet and 15 feet. Fairly tight turning radii shorten pedestrian crossings and inhibit reckless drivers from turning corners at high speeds. To allow for larger vehicles (i.e. fire trucks, moving vans, etc.) to turn corners, a 30' radius Clear Zone shall be established free of all vertical obstructions including but not limited to telephone poles, sign poles, fire hydrants, electrical boxes, site furnishings, etc. Bump outs shall be designed to handle utility vehicle loads appropriately.



B. Drive-Through's:

Drive-through service windows are permitted in the commercial center outright and village center in the rear or non-street side of the primary structure in mid-block and alley accessed locations provided they do not substantially disrupt pedestrian activity or surrounding uses.

C. Parks and Plazas:

Integral to the development, parks and plazas may be developed within the center of road turn-a-rounds or directly adjacent to. These parks and plazas will be excluded from the ROW and remain in Home Owners' Association control. Required ROW widths may be reduced where surrounded by roadway as long as minimum roadway design criteria are met. Utility easements may be provided outside the ROW to allow for provisions of full services.

D. Specialty Pavements:

Provisions of this PD shall allow for concrete pavement and/or pavers to be utilized at cross walks, commercial intersections and corner radii, in addition to asphalt pavement meeting or exceeding minimum design criteria outlined. Where pavers are utilized, they shall be designed as acceptable with the City of Kingsland.



Specific Criteria

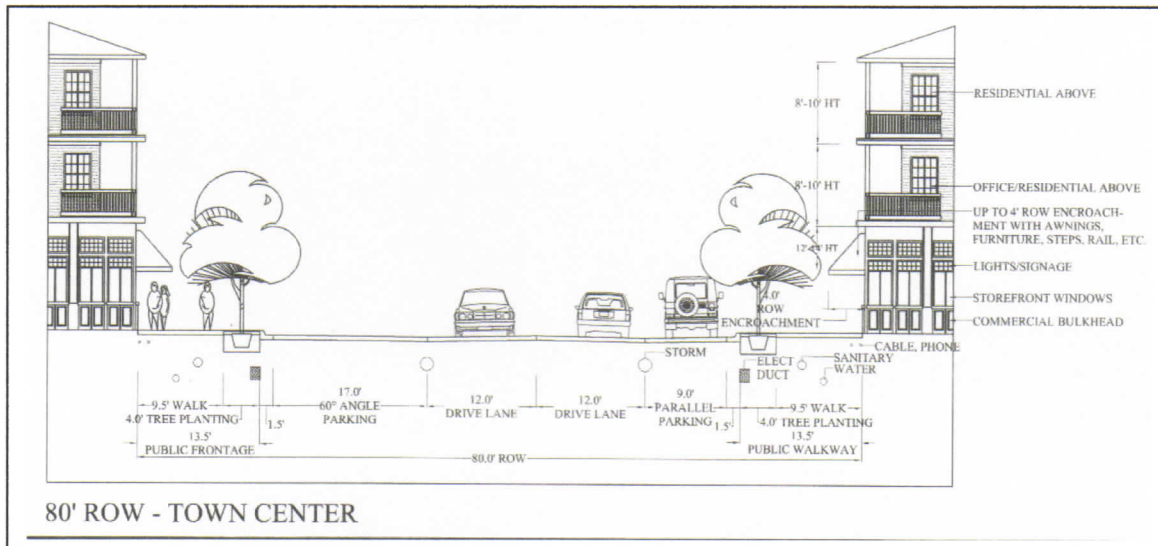
The following Transect Zone/Street Type Matrix establishes criteria for allowed roadway designs within each transect zone. The criteria are designed in compliance with Section 162, *Kingsland Zoning and Land Development Ordinance*, the 2006 International Fire code and, if discrepancies or misinterpretations occur, the 2006 version of the fire code shall prevail.

Shaded = Permitted Use (See notes) Not Shaded = Prohibited Use				
Transect Zones/ Street Type	Commercial Center	Village Center	Neighborhood General	Neighborhood Edge
80ft ROW Town Center				
80ft ROW Residential Blvd				
60ft ROW Residential St.				
50ft ROW Neighborhood St.				
20ft ROW Comm/Res Alley				
40ft ROW Residential Alley				

SPECIFIC ROADWAY PROVISIONS:

Spine Road – 80 foot plus ROW – Community Wide: A spine road may be provided that serves as the primary circulation route for the community. The design is to serve large traffic volumes and traffic flow takes precedence as compared with the other more pedestrian scale of streets identified within the community. Parking is not allowed on the streets side and suitable turn lanes, acceleration and deceleration lanes shall be provided as necessary. Road widths may increase to adequately handle larger traffic volumes and there may be multiple lanes. Only one spine road shall be allowed within this PD. The spine road is exempt from the regulations identified in this PD and shall comply with local and state criteria.

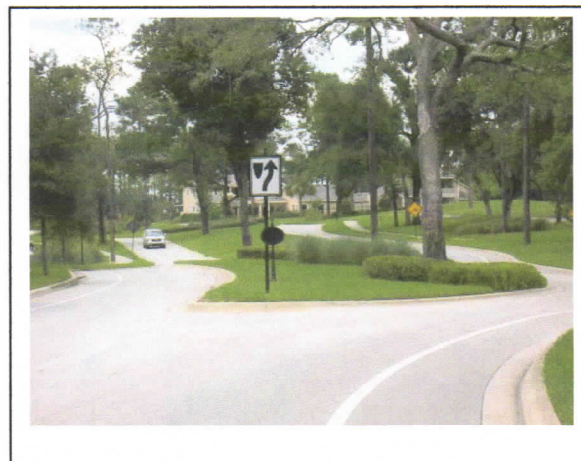
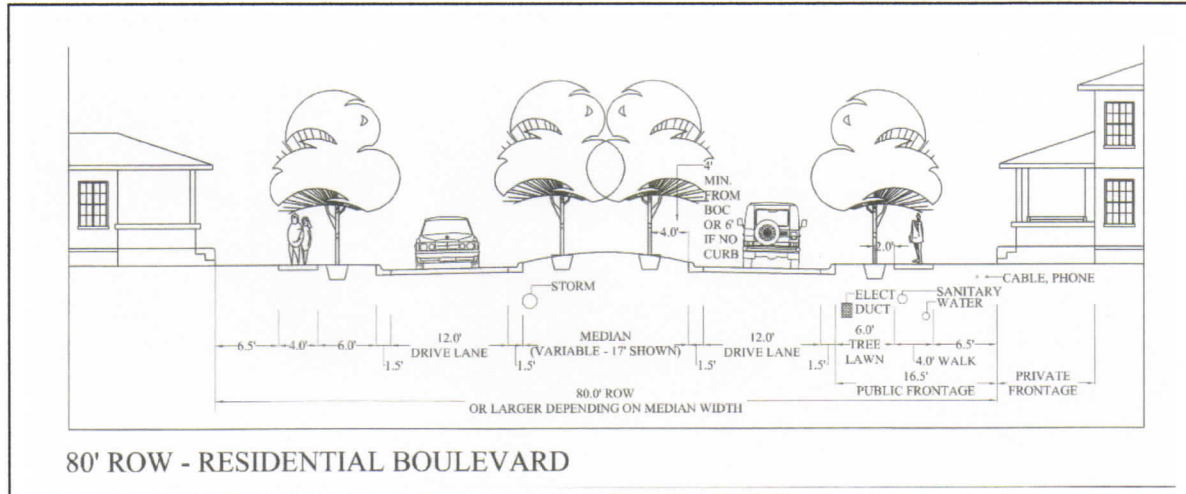
The 80 foot ROW – Town Center is the most important street in the town center. This ROW is lined with mixed use commercial buildings that are positioned along the ROW line. Parking, both parallel and angled are provided for and street trees shade the street and walk. Site furnishings are provided for within the main street and encroachments are allowed that provide for the interaction of the commercial uses with the street environment.



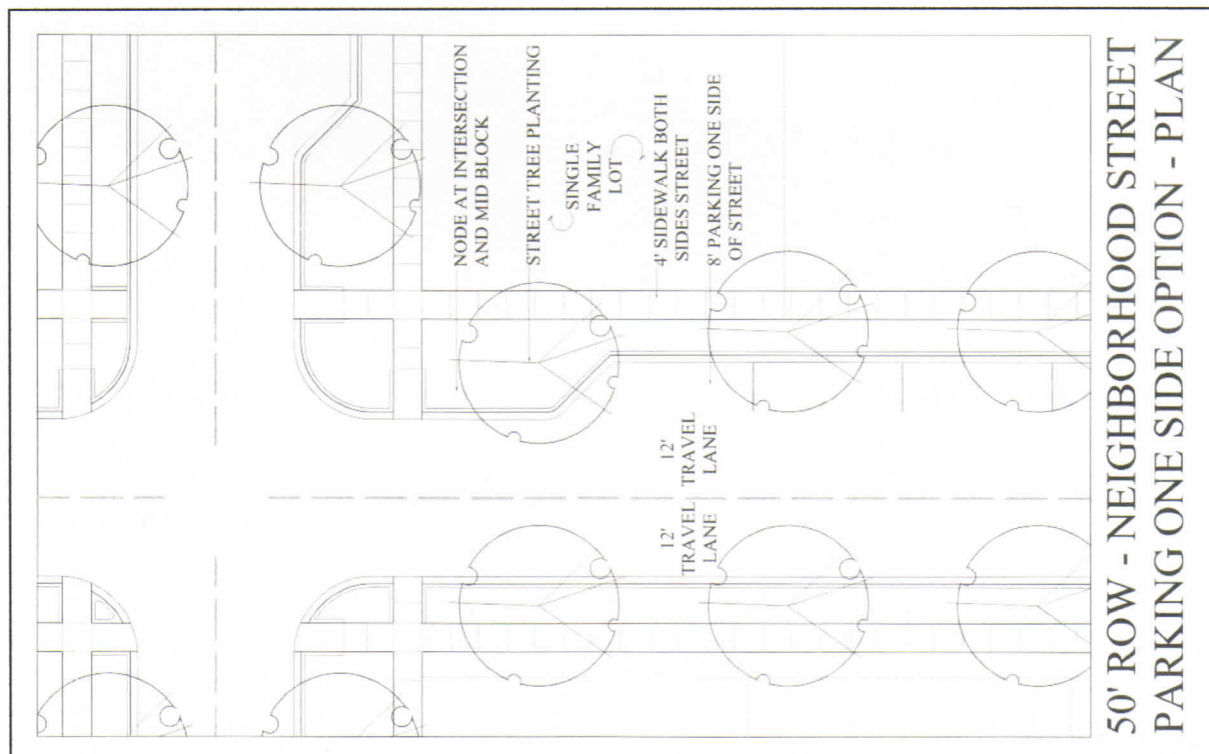
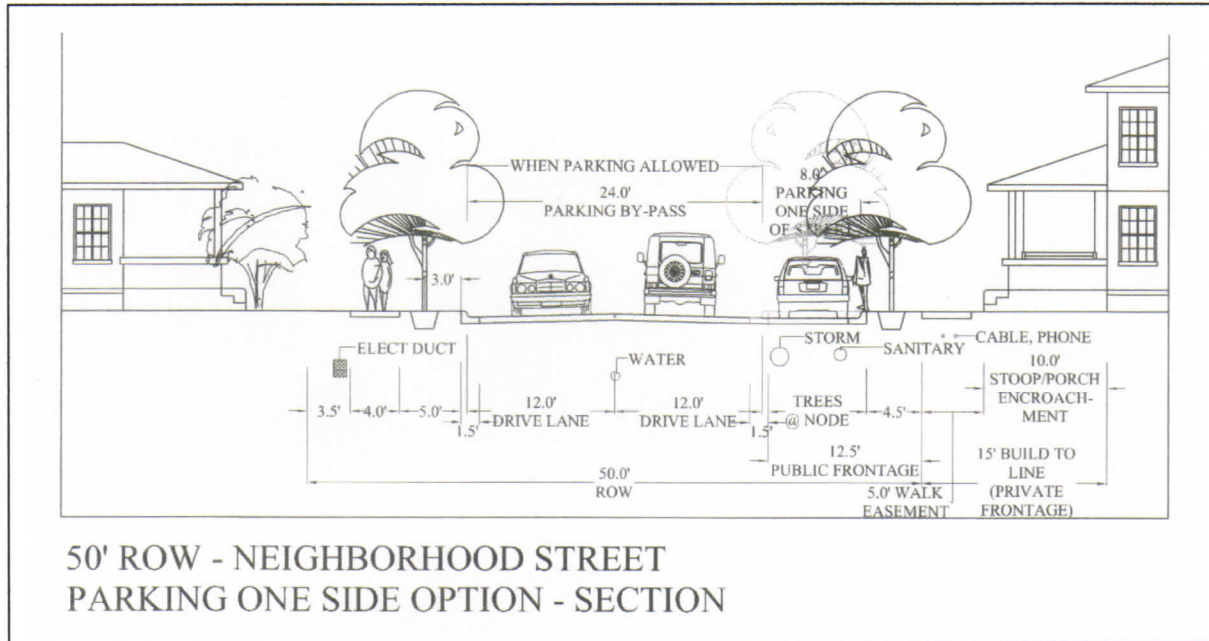
Representative 80' Town Center ROW

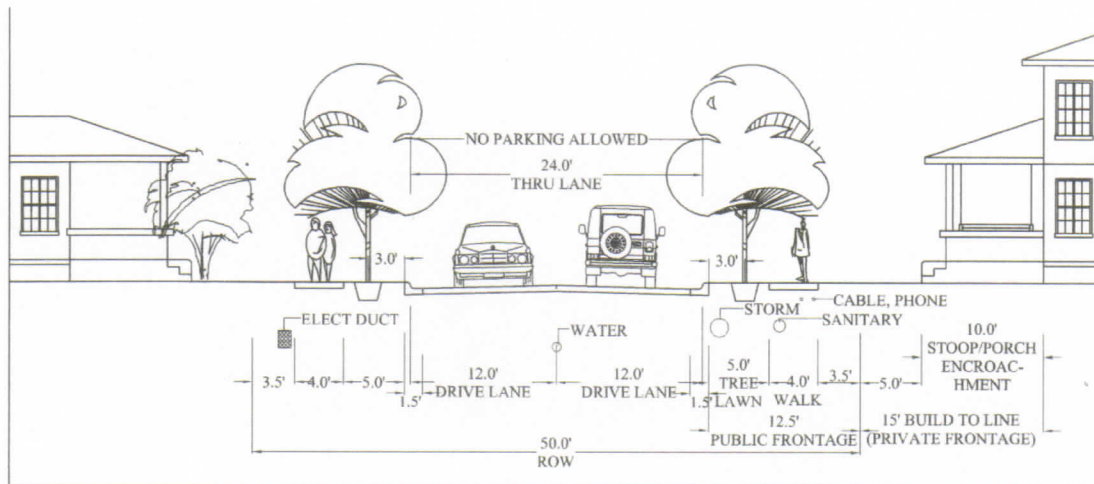


The 80 foot ROW – Residential Boulevard is the collector within the residentially zoned neighborhoods. This boulevard serves to distribute higher volumes of traffic to the representative neighborhoods. The boulevard integrates with parks and green space along the route and provides for tree lined streets with a planted median. Houses are allowed to front the boulevard.

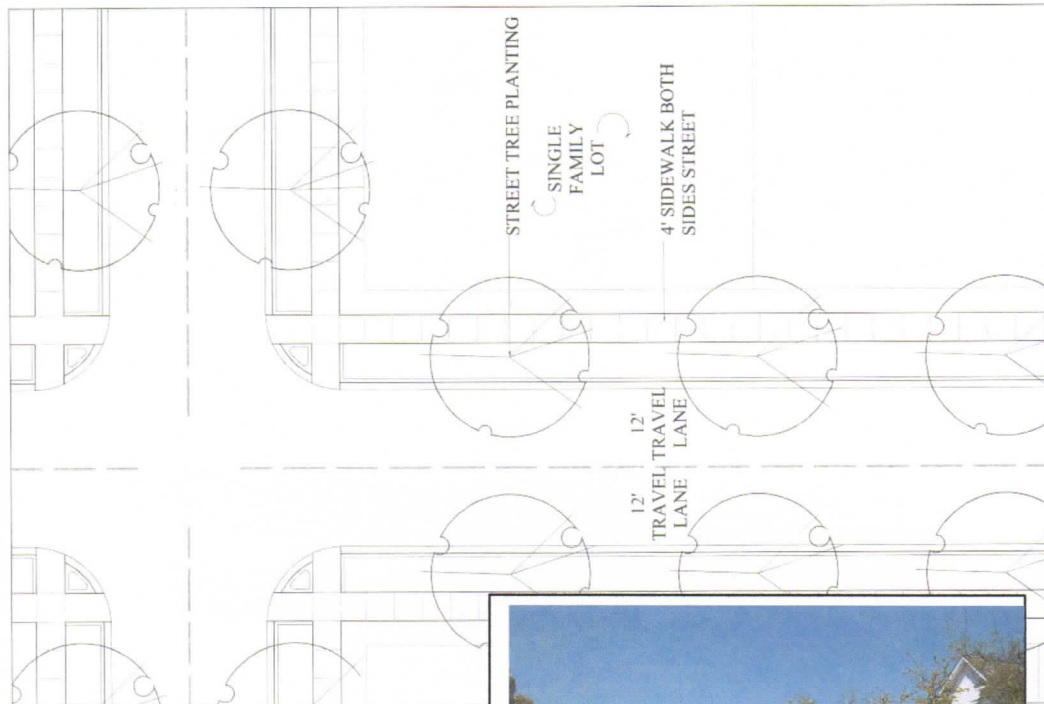


The 50 foot ROW – Residential Street is the residential scale neighborhood street that we remember walking as a child. This street allows for two lanes of traffic and parking along one side to help slow traffic. Street trees provide scale and shade to the road. Pedestrian walks/trails are safely separated from vehicle movements by planter strips that allow space for the planting of the street trees. Utilities are underground and integrated with the street design.





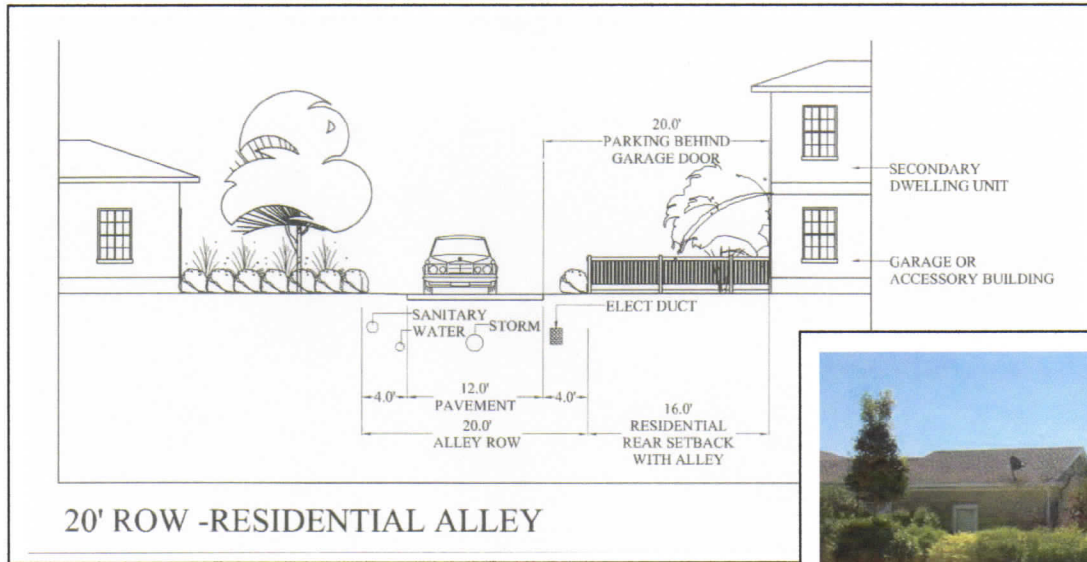
**50' ROW - NEIGHBORHOOD STREET
NO PARKING ALLOWED OPTION - SECTION**



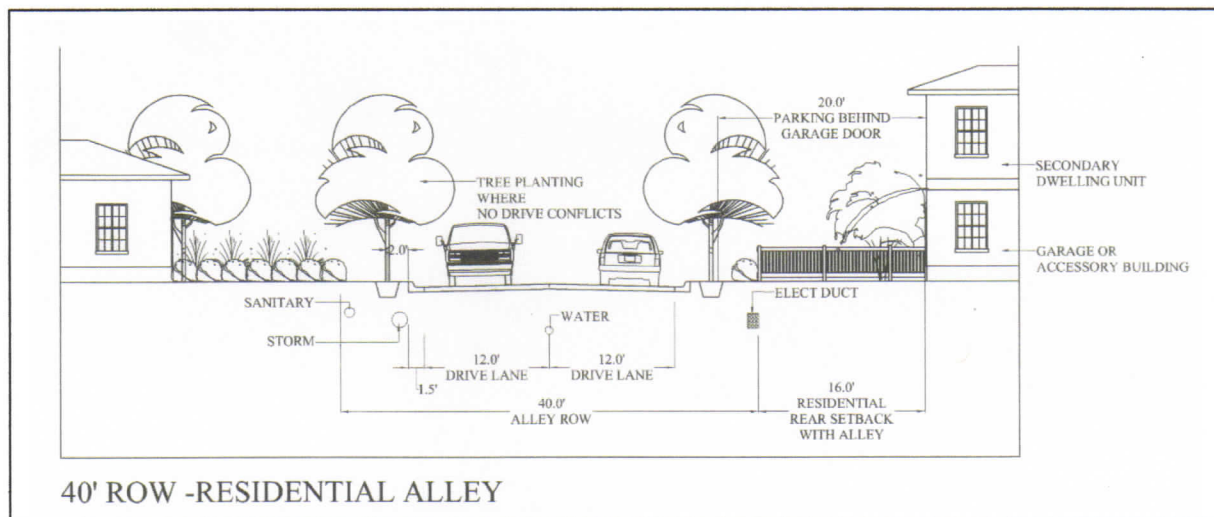
**50' ROW - NEIGHBORHOOD STREET
NO PARKING ALLOWED OPTION - PLAN**



Alleys serve the neighborhoods by providing rear access, which helps maintain a more social and unified pedestrian street corridor along the front side of the housing. Clutter is removed from the neighborhood when the street isn't dissected with driveways, refuse containers (and the spill over) are out of sight, utility boxes are also out of site and parked vehicles don't clutter the edge of streets. Alleys serve as the service and access points to the residences and provide for opportunities for houses to front natural or man-made features without having to look over large expanses of pavements. Two alley sizes are provided, the 20' ROW alley, where fire access is provided for on the front of the lot, the other, the 40' ROW alley, where the alley serves as fire access. Alleys are not allowed to dead-end without turnarounds meeting standard street design. Alley's may be dedicated public space or retained private similar to road dedications.



Typical Alley



AUTOMOBILE PARKING PER DISTRICT

Off-street automobile parking shall be provided for every lot per Section 54, Article V, General Provisions, *Kingsland Zoning and Land Development Ordinance*, current edition at date of this PD approval. Per Section 54, a parking space shall be 9 feet wide by 20 feet long. The size may be reduced where the space abuts a landscape island or a sidewalk where 4 foot clear walk space remains in residential areas and 6' clear walk space remains in commercial areas. In situations where the space abuts, the length may be reduced 2 feet to 18 feet in length while remaining 9 feet wide.

On-street automobile parking provided within the Commercial Center and Village Center may be counted towards the overall off-street parking requirements and reduce the off-street count accordingly. To receive credit for on-street parking, the parking must be located within 600' of the lot. The total number of on-street and off-street parking spaces must comply with the required amount as required per use in section 54. Parallel parking spaces must be a minimum 8 feet wide by 21 feet long. Angled parking is allowed complying with a minimum width of 9 feet wide by 20 feet long unless abutting a walk or landscape island as described above.

SIDEWALKS

The design and installation of sidewalks within Kingsland Royal Estates affects such factors as pedestrian and vehicular safety, pedestrian convenience, healthy living factors, community socialization, and the general appearance and livability of a community. The installation of sidewalks within Kingsland Royal Estates provides for a minimum amount of safety and connection and shall be provided as set forth in the following standards. All sidewalks shall comply with Section 176. Sidewalks, *Kingsland Zoning and Land Development Ordinance* with the exception of the required location of the walks shall be as outlined below. All required sidewalks shall be located in the public right-of-way or in a public access easement abutting the right-of-way unless otherwise approved by the City of Kingsland and this PD. Walk location in residential areas shall provide a minimum 4' landscape planting strip (6' or greater preferable) to allow for street tree planting.

Shaded = Sidewalk Requirements Apply

Transect Zones/ Sidewalk Requirements	Commercial Center	Village Center	Neighborhood General	Neighborhood Edge
Min. 5 ft. sidewalk on one side of public street or private driveway				
Min. 5 ft. sidewalk on both sides of a public street or private driveway				
Min. 6 ft. sidewalk (clear path) on both sides of a public street or private driveway				

Notes: Location of sidewalks shall be approved by the City Engineer.
Crosswalks at intersections shall meet ADA Standards.

3. *Section 172.1. Grades, Horizontal Curves, Tangents and Sight Distances.* Table Inset: Minimum length of tangent between reverse curves; Expressways, Freeways and Arterial Streets – 300', Collector Streets – 200' and Other Streets – 100'.

Variation to Section 172.1, Table Inset: Minimum length of tangent between reverse curves; Expressways and Freeways – 300', Arterial Streets and Collector Streets – 100', and Other Streets – 50'.

4. *Section 174.3. Minimum Pavement Width.* ... "Curb radius of intersection streets must have a minimum pavement width of 30'. All other street width requirements shall be determined by the Commission and the Council as required."

Variation to Section 174.3 Minimum Pavement Width: ... "Curb radius of intersection streets must have a corner curb radii shall be between 10 feet and 15 feet. Fairly tight turning radii shorten pedestrian crossings and inhibit reckless drivers from turning corners at high speeds. To allow for larger vehicles (i.e. fire trucks, moving vans, etc.) to turn corners, a 30' radius Clear Zone shall be provided. "

RESTRICTIVE COVENANTS

Restrictive covenants may govern additional use(s), architectural standards, the operation and maintenance of privately-owned property, facilities, amenities and other items. If utilized, covenants will be incorporated into appropriate legal documents at time of property transfer, prior to occupancy of any residential and/or commercial parcel. A homeowners and/or business association(s) will be created to administer and enforce these covenants. Each property owner will pay a capital contribution fee and annual dues to the property owners association.

These covenants and restrictions shall also make all buyers aware of the PD District, its regulations, the designations of permanent open space, as well as other restrictions imposed by this PD District. The common facilities within the development will be conveyed and maintained by a property owners association, their successors and assigns. Property owners will be legally required to adhere to develop their property in accordance with this PD District, covenants, and other applicable regulations as required by the City of Kingsland. In no way do restrictive covenants supersede existing code requirements as administered by the City of Kingsland and herewith in this PD.

PROPOSED DEDICATION OR RESERVATION OF LAND FOR PUBLIC USE

In accordance with the criteria established here within and within the *Kingsland Zoning and Land Development Ordinance*, Kingsland Royal Estates will include various dedicated and reserved elements for public and community use. Specific ownership arrangements of land reserved for public and community uses will be determined through the PD approval process.

There will be property within the development reserved for public and community uses, such as road ROW and parks. These properties will be transferred to the appropriate entity at a time to be determined by the developer and the City of Kingsland. The Regulating Plan(s) and subsequent platting of the development will further determine areas for these uses along with their ownership arrangement.

UTILITY PROVISIONS

The development of the property will utilize services provided to the site through the City Water and Sewer Department. Services on site will be provided within Public Right-of-ways and alleys at the cost of the development. All sizing, connections, tap fees, etc. will be coordinated and approved through the Kingsland Water and Sewer Department prior to installation. All utilities shall be underground and location below pavements shall be minimized. Lateral taps under streets should be avoided.

TRAFFIC CONTROL DEVICES

Traffic control devices, roadway signage, and roadway lighting should be an extension of the character of a community. The PD District permits the use of ornamental street signs, such as wooden or iron poles and/or frame signage for use within the boundaries of the PD District. All traffic control devices shall comply with applicable local, state, and/or federal standards.

SIGNS

Signs shall comply with Article XII. Signs, *Kingsland Zoning and Land Development Ordinance*.

LANDSCAPING AND TREE PRESERVATION

Landscaping and tree preservation shall comply with Article XIV, *Kingsland Zoning and Land Development Ordinance*

GLOSSARY

Words and phrases shall abide to the meanings as provided within *Article III, Definition of Terms used in this Ordinance* and additional definitions provided throughout the ordinance specific to particular Articles and Sections. The following definitions are in addition to those provided within the *Kingsland Zoning and Land Development Ordinance* to assist in providing clarity with the specific provisions contained here within. The following definitions are not intended to be all inclusive and should be deemed supplementary to the *Kingsland Zoning and Land Development Ordinance* only. If conflicts exist, those as defined within the *Kingsland Zoning and Land Development Ordinance* shall prevail.

Accessory Building:

A smaller, detached structure located in the rear of a lot. Accessory Buildings may be two stories in nature serving the following purposes: garage, home office, workshop, carriage houses and other accessory uses as allowed here within this PD.

Accessory Use:

An accessory use is one that is customarily incidental and subordinate to the principal use and located on the same lot with such principal use.

Build To Line:

The distance outlined that the structure indicated shall be built to.

Carriage Houses:

Carriage houses are secondary dwelling units are located on the same lot as the primary dwelling unit and may be rented but not sold separately. Carriage houses may be located within the primary building or within an Accessory Building to the rear of the primary building. Carriage houses shall not exceed 1,200 sf of living space. Carriage houses shall have separate outside entrances and only one carriage house is allowed per primary residence (2 total units).

Civic Use:

Occupied building space use primarily for neighborhood use, public education, charity, cultural performance, gatherings, recreation use, displays and accessory uses administered by government, non-profit neighborhood, cultural, educational, charitable and religious organizations.

Combined Side Yard Adjacent Lot Setback:

The sum of the distances between the side lot lines and the side elevations of a building. Roof overhangs may encroach into the setback. On street corners, the Side Street Setback shall be included in the calculation.

Commercial Center:

A district in which occupied building space used for the conduct of retail, professional services, personal services, workshop, office, artisan, restaurant, lodging, childcare, professional business, governmental services, entertainment, recreational uses and accessory uses. The intent is to serve highway users and well as the Kingsland Royal estates community. The permitted uses identified by the Article VI, Section 61.2.C-3, (C-3 Commercial District) within the Kingsland Zoning Code shall be govern allowable uses.

Façade

An elevation or “face” of a building, from ground level to roofline.

Front Façade:

The elevation with the main entrance to a building, usually facing a public street.

Home Based Business:

An occupation for gain or support conducted by a member(s) of a family residing on the premises and up to one non-family employee, entirely within the main dwelling. Home based businesses may include sales (not on premises), personal services (such as hair salon), professional services and offices. A minimum of 2 off-street parking spaces is required for home based businesses..

Live/Work Unit:

A building in single ownership that provides commercial uses on the first floor and a residential use dwelling unit on the upper floor(s). The owner may occupy the commercial space and reside in the dwelling unit, or the owner may lease or rent the commercial space or the dwelling together or separately.

Mixed-Use Building:

A single building that occupies two or more of the following use: retail, professional services, personal services, workshop, office, artisan, restaurant, lodging, childcare, professional business, governmental services, entertainment, recreational, residential and related accessory uses. Uses may be divided vertically or horizontally.

Office Use:

Premises used for services, including professional, financial, clerical, administration, medical and accessory uses.

Point-Of-Service:

Office uses or services frequented by customers or clients.

Primary Building:

The principal building on a site, containing the primary residence or business. The primary building fronts on the primary street.

Primary Street:

When there are two streets abutting a lot, the primary street will be the street identified by the building's legal address.

Primary Street Setback:

The distance between the frontage line of a primary street and the front façade of a building

Rear Setback:

The distance between the rear lot line and the rear wall of a building. Roof overhangs may encroach into the setback.

Residential Use:

Premises or dwelling used primarily for human habitation and related accessory uses. There are three classifications applicable to residential uses as outlined within Kingsland Royal Estates as follows:

- **Residential Single Family R-1** – Allowable uses as identified within Article VI, 60.1.2 and 60.1.3, Kingsland Zoning Ordinance. Manufactured housing is allowed when assembled on foundation on-site, and meets Earthcraft house energy design criteria or a HERS rating of 70 or less. Mobil homes are not allowed.
- **Residential Single Family R-2** – Allowable uses as identified within Article VI, 60.2.2 and 60.2.3, Kingsland Zoning Ordinance. Manufactured housing is allowed when assembled on foundation on-

site, and meets Earthcraft house energy design criteria or a HERS rating of 70 or less. Mobil homes are not allowed.

- **Residential Multi Family R-3** - Allowable uses as identified within Article VI, 60.3.2 and 60.3.3, Kingsland Zoning Ordinance. Manufactured housing is allowed when assembled on foundation on-site, and meets Earthcraft house energy design criteria or a HERS rating of 70 or less. Mobil homes are not allowed.

Retail Use:

Premises used for the exchange of services or goods and accessory uses including those permitted uses identified by the Article VI, Section 61.2.C-3, (C-3 Commercial District) within the Kingsland Zoning Code. Additionally, uses that involve the sale of alcoholic beverages for consumption on-premise and the sale of alcoholic beverages for consumption off-premises shall be allowed in compliance with other local and state liquor license regulations.

Side Street:

When there are two streets abutting a lot, the primary street will be the street identified by the building's legal address and the side street will be the other abutting street.

Side Street Setback:

The distance between the frontage line of a side street and the street facing side elevation of the building. Roof overhangs may encroach into the setback.

Storefront:

The portion of a building at the first story of a building that is made available for retail use. Storefronts shall be directly accessible from sidewalks.

Stoop:

An entry platform on the frontage of a building. Stoops may be roofed but they need not necessarily be enclosed. Stoops that encroach into the build to line are not allowed to be enclosed.

Townhouse:

Attached single-family houses on individual platted lot. A townhouse shares a common wall with one adjacent unit.

Workshop Use:

Premises used for the creation, assemblage, repair of goods and hobbies, including their retail sale, unless otherwise prohibited. Workshop use does not include automotive service or repair. Workshop uses shall be completely enclosed within a building.

Attachment A

Legal Description and Boundary Map

Tax Parcels 057-004 & a portion of 070-058

All that tract of land lying and being in the 1606th G.M.D., Camden County, Georgia and being more particularly described as follows:

BEGINNING at a concrete monument found as shown on a map of a Boundary Survey for LRD, LLC prepared by Cumberland Land Surveyors, dated March 28, 2006, and last revised June 26, 2007, said concrete monument indicated the southeastern corner of the subject property and lies North 63 degrees 16 minutes 29 seconds west a distance of 34.84 feet from the northerly right-of-way margin of Oakwell Church Road (80' R/W), From the POINT OF BEGINNG as thus established,

Proceed North 08 degrees 31 minutes 07 seconds east a distance of 1291.18 feet to a concrete monument found;

Thence south 86 degrees 11 minutes 27 seconds west a distance of 3004.81 feet to a concrete monument found;

Thence south 66 degrees 46 minutes 46 seconds west a distance of 661.50 feet to a concrete monument found;

Thence North 00 degrees 48 minutes 22 seconds west a distance of 2947.34 feet to a concrete monument found;

Thence North 00 degrees 49 minutes 11 seconds west a distance of 4062.48 feet;

Thence North 88 degrees 51 minutes 33 seconds west a distance of 1651.88 feet;

Thence North 07 degrees 16 minutes 57 seconds west a distance of 103.94 feet to a point along the southern right-of-way margin of Georgia Highway 40 (100'R/W);

Thence, continue along said right-of-way, North 84 degrees 32 minutes 07 seconds east a distance of 4241.76 feet;

Thence, leaving said right-of-way, south 30 degrees 03 minutes 21 seconds west a distance of 54.98 feet;

Thence south 76 degrees 33 minutes 13 seconds west a distance of 851.64 feet;

Thence south 38 degrees 01 minutes 01 seconds east a distance of 1516.09 feet;

Thence south 50 degrees 14 minutes 20 seconds west a distance of 1219.30 feet;

Thence south 37 degrees 46 minutes 26 seconds east a distance of 1259.23 feet to a concrete monument found

Thence south 39 degrees 28 minutes 57 seconds east a distance of 1988.61 feet to a concrete monument found;

Thence North 72 degrees 30 minutes 04 seconds east a distance of 1331.00 feet to a concrete monument found;

Thence south 22 degrees 36 minutes 56 seconds west a distance of 2294.56 feet to a concrete monument found;

Thence south 59 degrees 05 minutes 20 seconds east a distance of 1688.87 feet to a point on the northern right-of-way margin of Oakwell Church Road (80' R/W);

Thence continue along said right-of-way the following courses and distances:

south 65 degrees 14 minutes 38 seconds west a distance of 174.17 feet;
south 66 degrees 22 minutes 00 seconds west a distance of 183.24 feet;
south 70 degrees 46 minutes 08 seconds west a distance of 193.27 feet;
south 75 degrees 10 minutes 16 seconds west a distance of 100.32 feet;
south 66 degrees 42 minutes 19 seconds west a distance of 506.33 feet;
south 58 degrees 14 minutes 22 seconds west a distance of 900.54 feet;
south 54 degrees 14 minutes 42 seconds west a distance of 232.19 feet;

Thence , leaving said right-of-way margin, proceed North 63 degrees 16 minutes 29 seconds west a distance of 34.84 feet, to a concrete monument found, said point being the POINT OF BEGINNING.

Said tract or parcel of land contains 550.15 acres comprised of Tract 1 and Tract 3 as illustrated on a Plat of a Boundary Survey for LRD, LLC prepared by Cumberland Land Surveyors, dated March 28, 2006, and last revised June 26, 2007. Survey is hereby incorporated by reference.

Attachment B

Illustrative Site Plan

The following site plan was created for marketing purposes and to illustrate one of many ways the regulations in this PD document may be applied. The illustrative site plan is not part of this PD document other than a sample of how the regulations may be applied, therefore does not bind the property to the proposed plan.

KINGSLAND ROYAL ESTATES

ILLUSTRATIVE SITE PLAN FOR A MIXED USE COMMUNITY

Kingsland, Georgia

